

DEPARTMENT OF PLANNING AND PERMITTING  
**CITY AND COUNTY OF HONOLULU**

650 SOUTH KING STREET, 7TH FLOOR • HONOLULU, HAWAII 96813  
PHONE: (808) 768-8000 • FAX: (808) 768-6041  
DEPT. WEB SITE: [www.honolulu.gov](#) • CITY WEB SITE: [www.honolulu.gov](#)

MUFU HANNEMANN  
MAYOR



DAVID K. TANOUE  
DIRECTOR


ROBERT M. SUMITOMO  
DEPUTY DIRECTOR

April 6, 2009

2008/ELOG-1701 (df)

**ENGINEERING AND POLICY MEMORANDUM NO. CEB-1-09**

TO: ENGINEERS, DEVELOPERS, UTILITY COMPANIES, GOVERNMENT AGENCIES, CONTRACTORS, AND OTHER INTERESTED PARTIES

FROM: DAVID K. TANOUE, DIRECTOR   
DEPARTMENT OF PLANNING AND PERMITTING

SUBJECT: JOINTING REQUIREMENTS FOR CONCRETE SIDEWALKS AND DRIVEWAYS

This policy memorandum is intended to improve the appearance and integrity of all concrete sidewalks and reinforced concrete driveway aprons constructed in the City's rights-of-way by minimizing the potential for cracking of the concrete. It was developed as a result of recommendations from the Cement and Concrete Products Industry of Hawaii and consultation with the Departments of Design and Construction and Facility Maintenance.

**This policy memorandum shall apply to work within the City and County of Honolulu only.**

Effective July 1, 2009, the following shall apply to the construction of new concrete sidewalks and driveways:

1. The construction of score lines will no longer be required.
2. Expansion joints shall be constructed at fixed structures, such as buildings, bridges, or walls and at the beginning and end of curb returns. The joints shall be constructed with premolded expansion-type filler and filled with joint sealer.
3. Divide all sidewalks between expansion joints into sections approximately five (5) feet in length by providing transverse contraction joints.

Whenever sidewalks are greater than seven (7) feet in width, longitudinal contraction joints shall be constructed such that they intersect the transverse contraction joints to form secure uniform blocks that are approximately square.

4. Divide all driveway aprons with contraction joints that are transverse to the roadway, equally spaced and no greater than five (5) feet apart. The contraction joints shall extend through the ADA-accessible route. In addition, a contraction joint shall be provided along the intersection of the driveway apron slab and the ADA-accessible route. If necessary, contraction joints parallel to the roadway shall be constructed in the driveway apron such that they intersect the transverse contraction joints to form secure uniform blocks that are approximately square.
5. Score lines shown on Sheets R-7, "Driveway Apron with Rolled Curb" and R-27, "Full-Width Sidewalk at Curb Return" of the Standard Details for Public Works Construction September 1984 shall be disregarded. Instead, transverse and longitudinal contraction joints and expansion joints shall be provided in accordance with this policy.
6. Standard Detail R-29A, dated April 2000, is hereby superseded with Standard Detail R-29A dated July 2009 (see attached). As a reminder, the detail is intended to be a guide. A site-specific detail complying with this policy may be required.
7. The contraction joint detail shown on Standard Detail R-29A, dated July 2009, shall also apply for sidewalk construction. Contraction joints may be constructed either by forming or saw-cutting.

To reflect these changes, revisions to the applicable sections of the "Standard Specifications for Public Works Construction September 1986" are as follows:

**Section 42.3.C:** Delete the section in its entirety and replace it with the following:

*"C. Placing and Finishing. Prior to pouring of concrete, the subgrade shall be dampened. Concrete shall be poured continuously and shall be thoroughly tamped and floated to a smooth and even surface. The pouring and constructing of alternating blocks shall not be permitted."*

**Section 42.3.D:** Delete the section in its entirety and replace it with the following:

*"D. Joints. Expansion joints shall be constructed at fixed structures, such as buildings, bridges, or walls, and at the beginning and end of curb returns. The joints shall be constructed with ½" thick premolded expansion-type filler, extending from the bottom of the sidewalk to approximately ½" below the top of the sidewalk, then filled with joint sealer."*

*Divide sidewalk between expansion joints into sections approximately five (5) feet in length by providing transverse contraction joints.*

*Where sidewalks are more than seven (7) feet in width, provide longitudinal contraction joints that intersect the transverse contraction joints to obtain secure uniform blocks that are approximately square.*

*The width of contraction joints shall be at least 1/8", but not greater than 3/16".*

*Construction shall be as specified under Section 37, 'Portland Cement Concrete Pavement'.*

**Section 46.3.C:** Delete the section in its entirety and replace it with the following:

*"C. Placing and Finishing. Before pouring concrete, the subgrade and the forms shall be prepared in conformity with the requirements under Section 42, 'Concrete Sidewalks.'*

*The concrete shall be placed on the prepared subgrade. The reinforcing mesh shall be placed at mid-depth of the driveway apron. The concrete shall be floated to a smooth and even surface.*

*The finished surface of aprons shall be broom-finished perpendicular to the curb. For monolithic construction of curb and driveway apron, a contraction joint shall be constructed as shown on the Standard Details."*

**Section 46.3.D:** Add a new subsection D as follows:

*"D. Joints. Divide the driveway apron with contraction joints transverse to the roadway that are equally spaced and no greater than five (5) feet apart. The contraction joints shall extend through the ADA-accessible route. In addition, a contraction joint shall be provided along the intersection of the driveway apron slab and the ADA-accessible route. If necessary, contraction joints parallel to the roadway shall be constructed in the driveway apron such that they intersect the transverse contraction joints to form secure uniform blocks that are approximately square.*

*The width of contraction joints shall be at least 1/8", but not greater than 3/16".*

*Construction shall be as specified under Section 37, 'Portland Cement Concrete Pavement'.*

Contractors shall be responsible for assigning qualified flatwork finishers possessing the proper license, certification, job classification, skill, training, and/or experience to properly perform the work specified in accordance with this policy.

Flatwork is defined as any concrete work that requires tools or machines to be used during the placement and finishing operations of concrete. Concrete flatwork includes concrete work that requires a specified finishing, smoothness, or rigid surface tolerances, which includes, but is not limited to, sidewalks, walkways, driveways and road pavements.

If there are any questions, please contact Mr. Don Fujii of the Site Development Division at 768-8107.

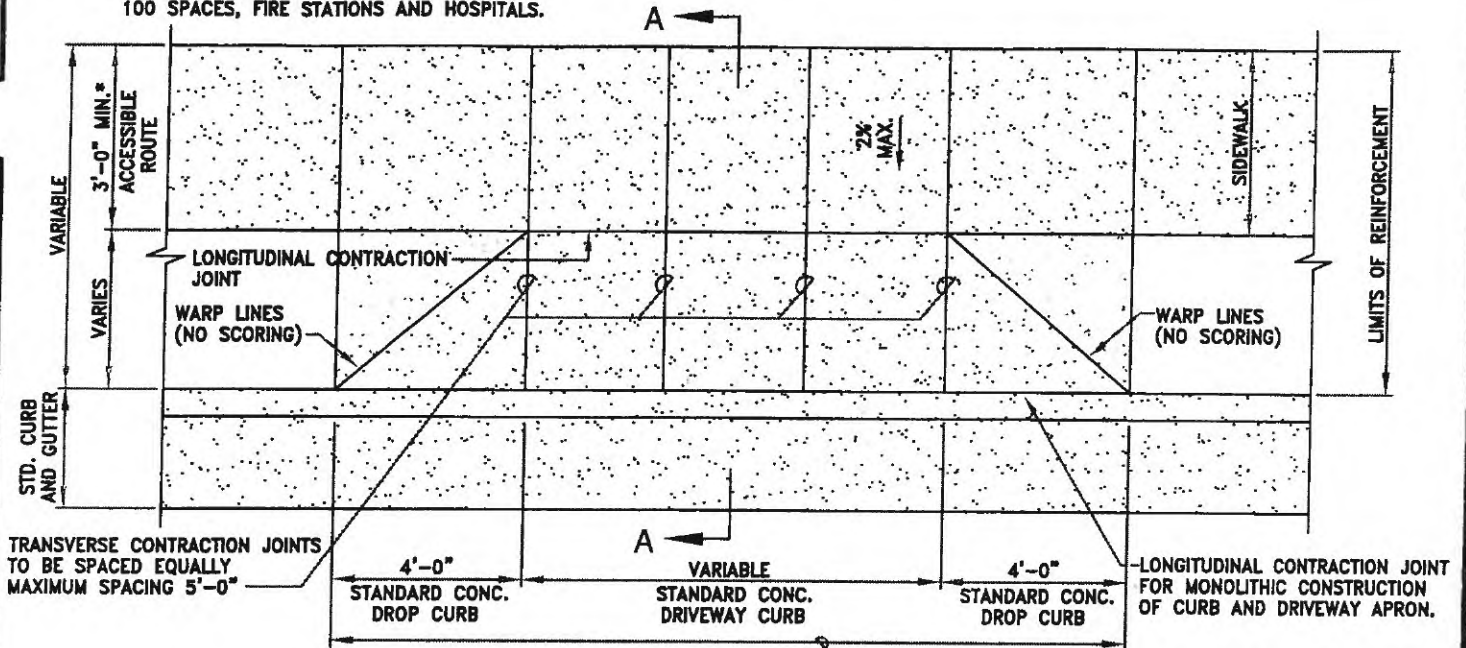
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Attachment

cc: CEB-PIS

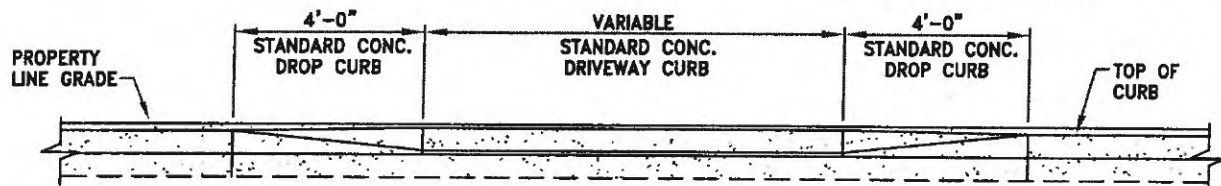
Department of Public Works - Counties of Hawaii, Maui and Kauai  
Cement and Concrete Products Industry of Hawaii

CURB RETURN TYPE DRIVEWAY MAY BE PERMITTED FOR PARKING AREAS EXCEEDING 100 SPACES, FIRE STATIONS AND HOSPITALS.



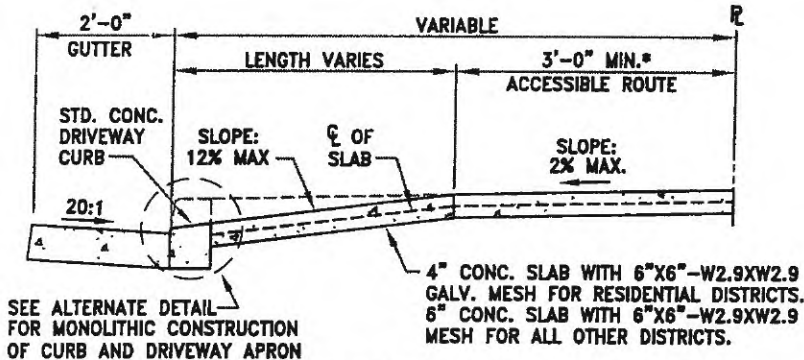
PLAN  
NO SCALE

LIMITS OF REINFORCEMENT. 4" THICK CONCRETE REINFORCED WITH 6"x6"-W2.9XW2.9 GALV. WELDED WIRE FABRIC FOR RESIDENTIAL DISTRICTS. 6" THICK FOR ALL OTHER DISTRICTS. SURFACE SHALL BE BROOM FINISHED.

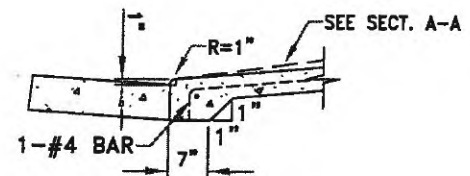
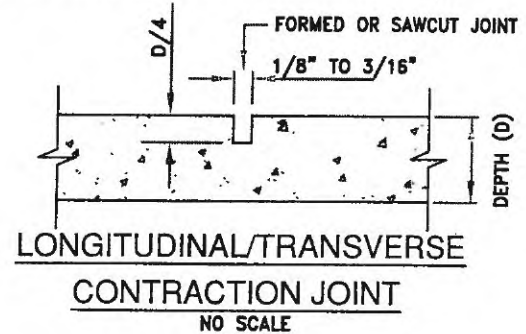


ELEVATION  
SCALE: 1/4"=1'-0"

\* MATCH WIDTH OF EXISTING SIDEWALK WHENEVER POSSIBLE



SECTION A-A  
SCALE: 3/8"=1'-0"



ALTERNATE DETAIL  
SCALE: 3/8"=1'-0"

NOTES:

1. FOR NEW SUBDIVISIONS, PROVIDE CENTERLINE ROADWAY STATIONING TO THE CENTERLINE OF THE DRIVEWAY (APPLICABLE TO STD. DET. R-7).
2. THIS DETAIL IS FOR GUIDANCE ONLY. A DESIGN DETAIL MAY BE REQUIRED FOR EACH SPECIFIC SITE. THE DETAIL MUST COMPLY WITH THE REQUIREMENTS SHOWN.
3. THE FINISHED SURFACE OF APRONS SHALL BE BROOM FINISHED PERPENDICULAR TO CURB LINE.
4. THE LONGITUDINAL AND TRANSVERSE CONTRACT JOINTS ARE SHOWN FOR GUIDANCE ONLY. JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ENGINEERING AND POLICY MEMORANDUM NO. CEB-1-09.

CITY & COUNTY  
OF HONOLULU

DRIVEWAY APRON

SCALE: AS NOTED

JULY 2009

STANDARD  
DETAILS

R-29A